

Land North East of Blandford Forum

Framework Masterplan

Savills

3rd February 2016

Growth strategy

A sustainable urban extension north east of Blandford continues a historic growth pattern for the town.

Blandford marks an important crossing of the River Stour with its settlement origins established in the 12th century within a loop of the river. The town evolved into an important Georgian centre, positioned along an historic route between Weymouth and London.

The formation of the Great Western Turnpike between Salisbury and Axminster (Salisbury Road) in the 18th century established a clear focus for growth along this major radial route, with ribbon development extending in a north east direction.

Growth and expansion gradually continued along this corridor throughout the 19th and 20th century with the arrival and subsequent closure of the railway. During this time, town growth occurred within both Blandford town and Pimperne parishes, with the parish boundary line previously being positioned much closer to Blandford town centre.

The bypass road, constructed during the 1980's and opened in 1992, enclosed Blandford on three quarters of the town periphery to the south east and north. An artificial 'town collar' was formed, altering the growth pattern.

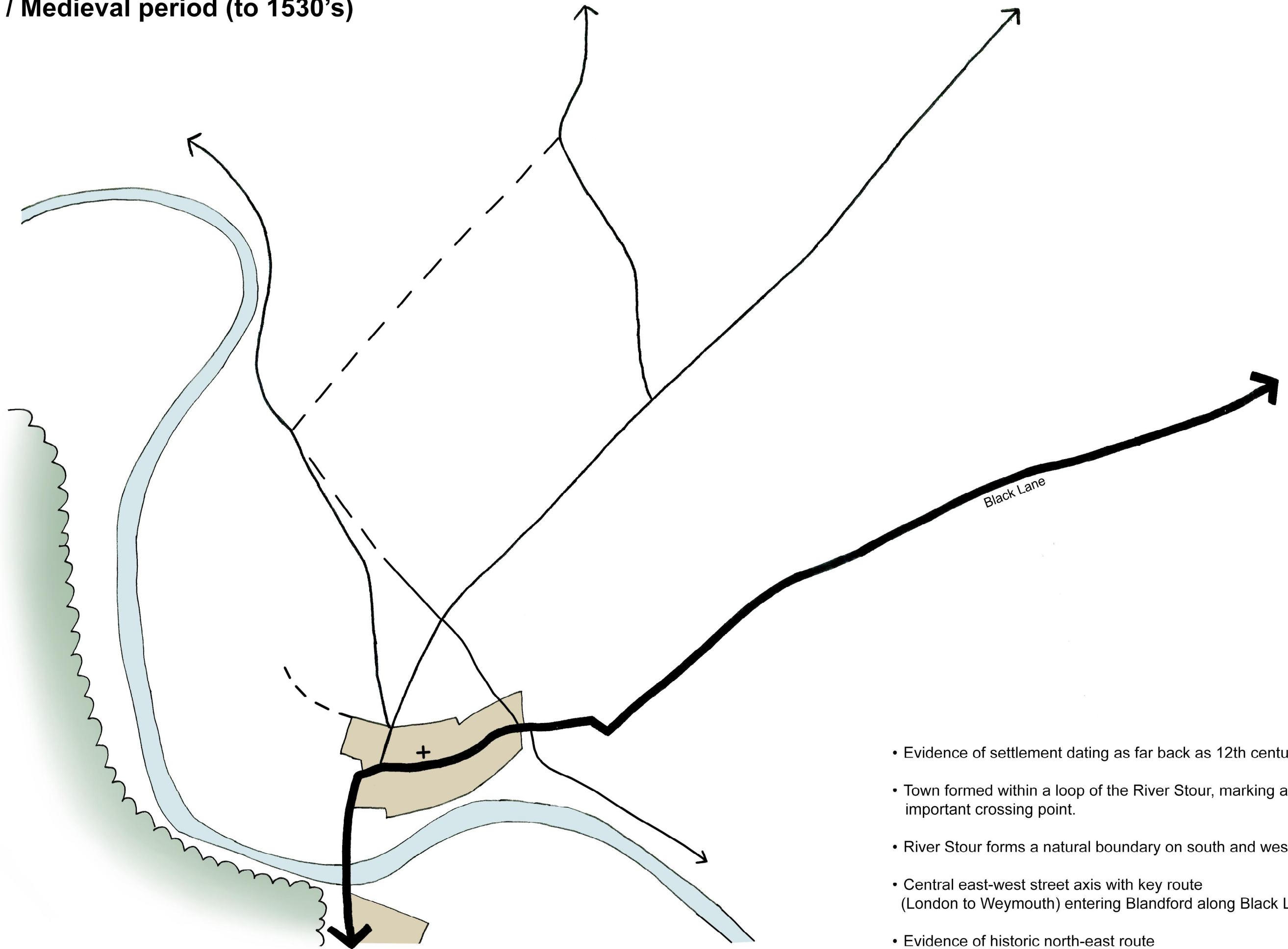
Suburban housing estates (typically curvilinear cul-de-sacs) were developed on higher ground to the north and east, infilling around the fringes of the bypass route.

Land north east of Blandford presents the opportunity to create a new town gateway for Blandford, which reinforces the historic growth pattern and the strategic importance of Salisbury Road as a focal point for both new and existing communities.

This document presents a framework masterplan response for the land north east of Blandford. The plans and information presented have been prepared in context to the information provided at this point (3rd February 2016). Further testing and technical review work should continue to progress the masterplan and confirm the concept presented in this document.

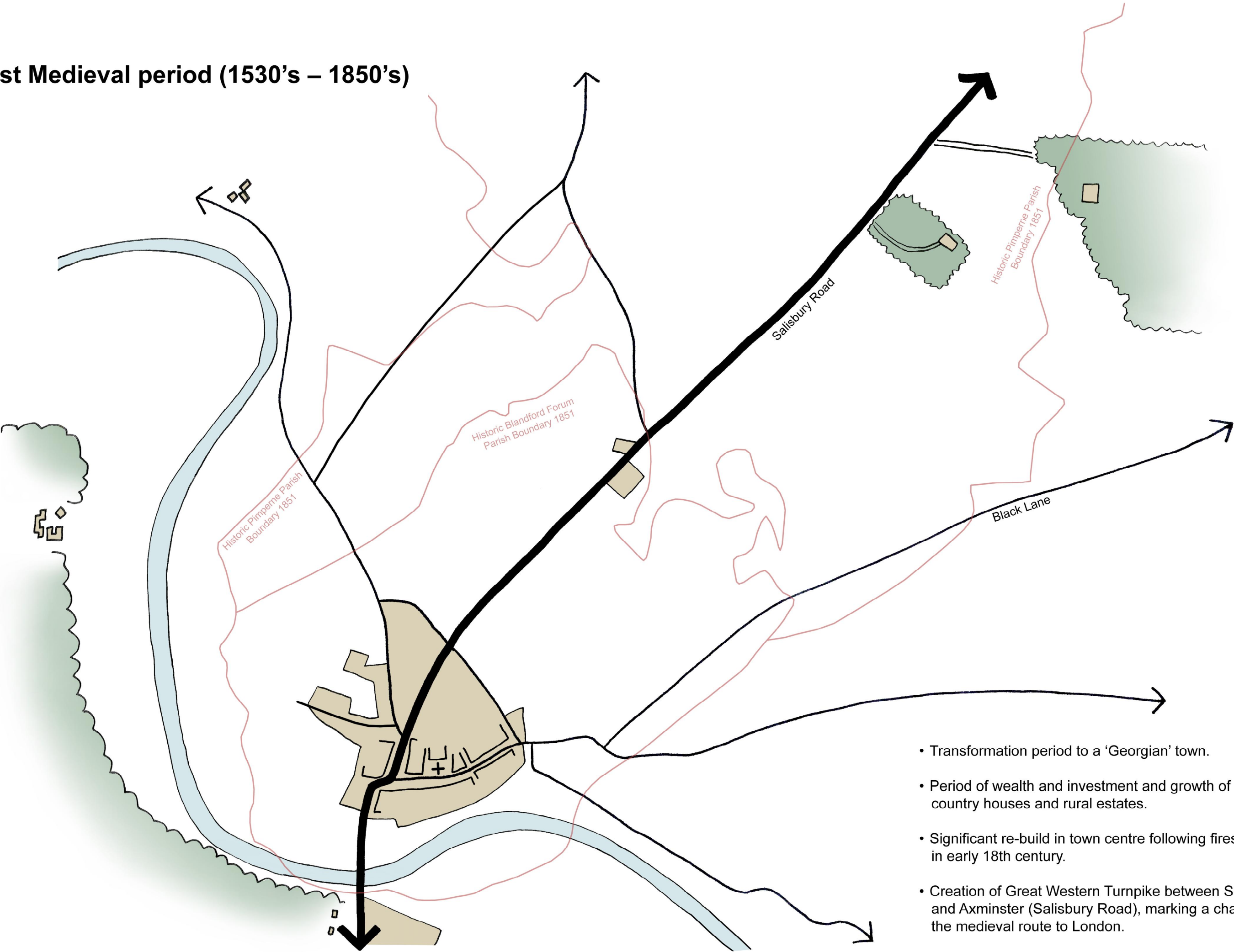


Origins / Medieval period (to 1530's)



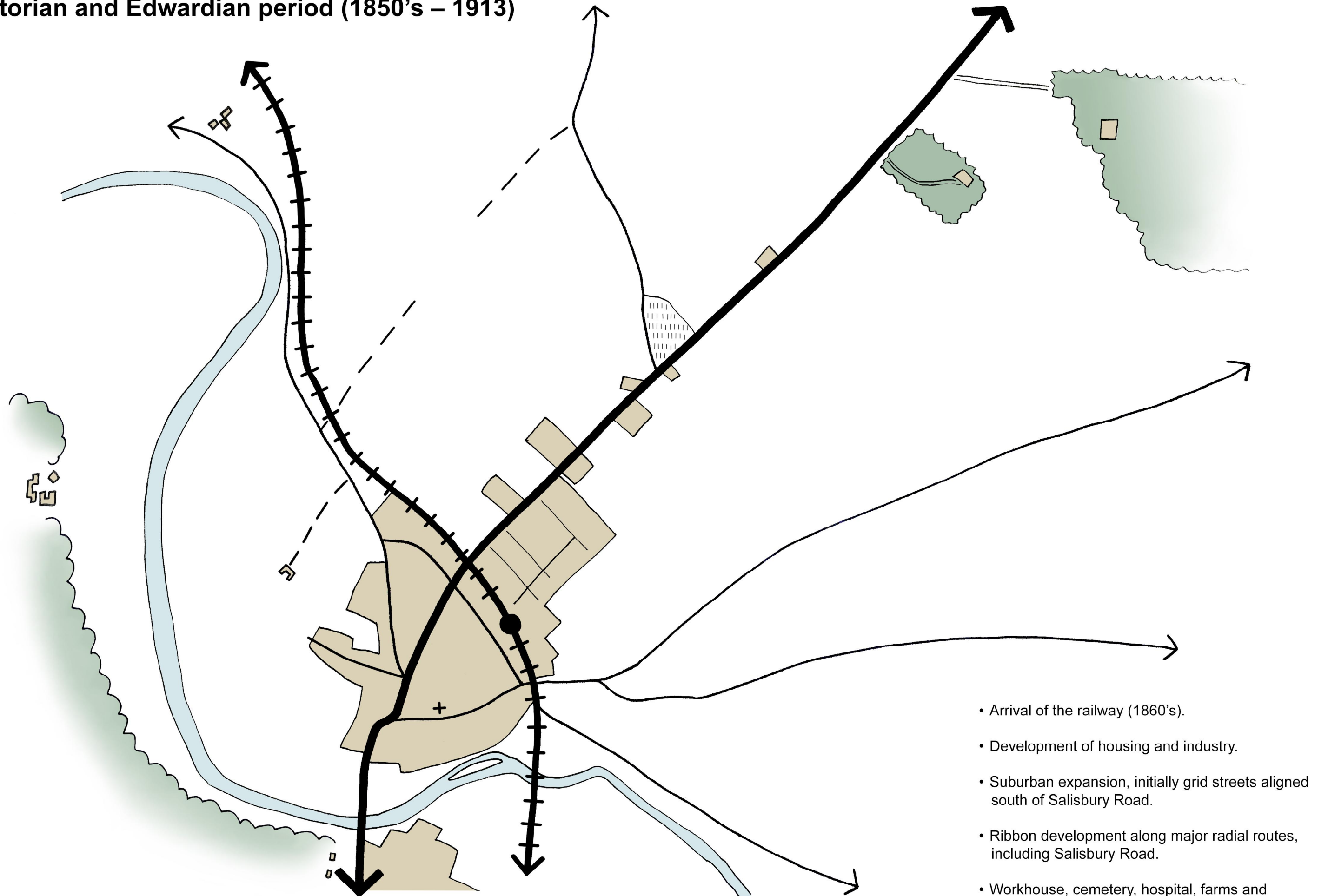
- Evidence of settlement dating as far back as 12th century.
- Town formed within a loop of the River Stour, marking an important crossing point.
- River Stour forms a natural boundary on south and west sides.
- Central east-west street axis with key route (London to Weymouth) entering Blandford along Black Lane.
- Evidence of historic north-east route (Salisbury Road alignment).

Post Medieval period (1530's – 1850's)



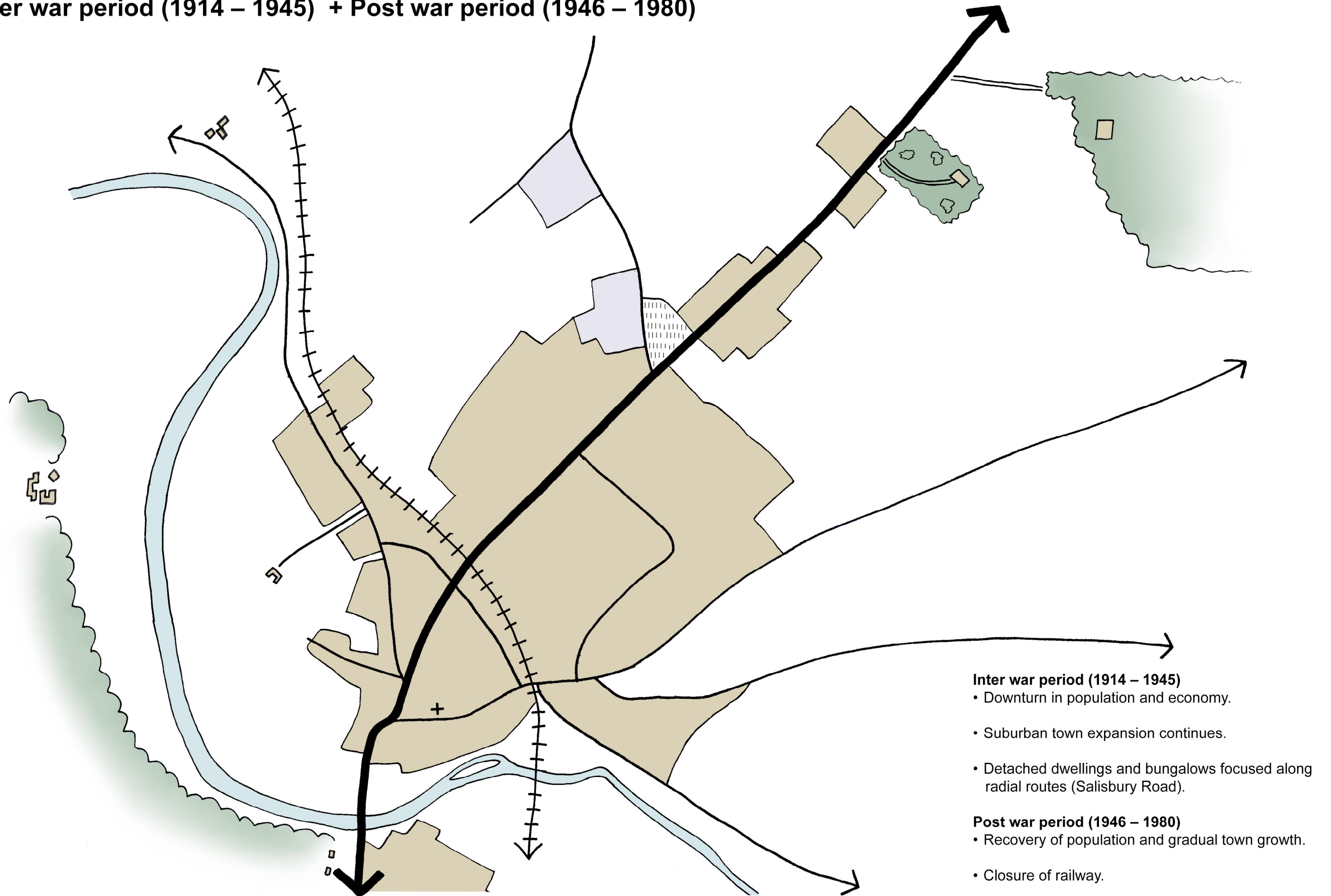
- Transformation period to a 'Georgian' town.
- Period of wealth and investment and growth of country houses and rural estates.
- Significant re-build in town centre following fires in early 18th century.
- Creation of Great Western Turnpike between Salisbury and Axminster (Salisbury Road), marking a change from the medieval route to London.
- Parish boundary previously closer to the town centre.

Victorian and Edwardian period (1850's – 1913)



- Arrival of the railway (1860's).
- Development of housing and industry.
- Suburban expansion, initially grid streets aligned south of Salisbury Road.
- Ribbon development along major radial routes, including Salisbury Road.
- Workhouse, cemetery, hospital, farms and cottages along Salisbury Road.

Inter war period (1914 – 1945) + Post war period (1946 – 1980)



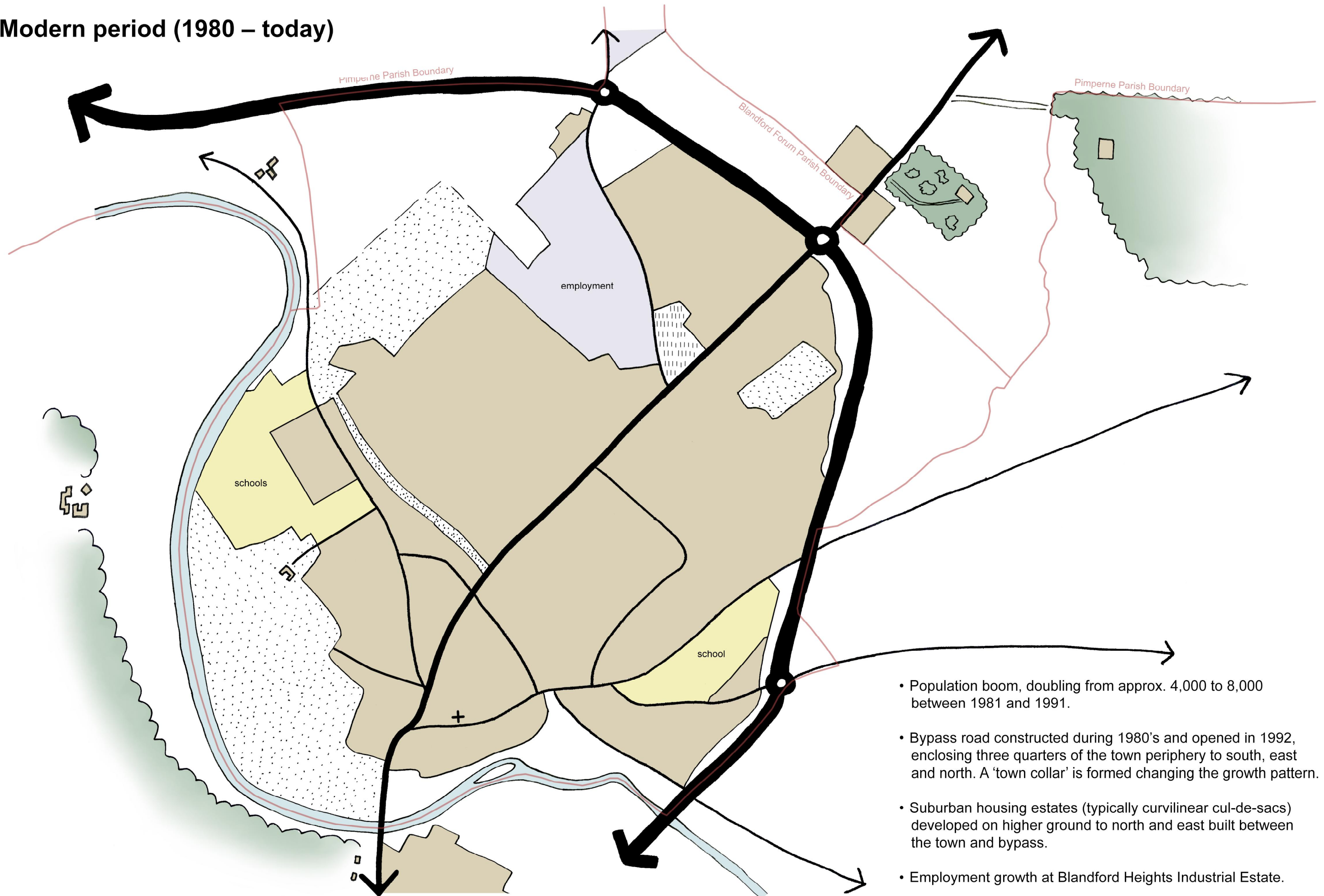
Inter war period (1914 – 1945)

- Downturn in population and economy.
- Suburban town expansion continues.
- Detached dwellings and bungalows focused along radial routes (Salisbury Road).

Post war period (1946 – 1980)

- Recovery of population and gradual town growth.
- Closure of railway.
- Growth continues along Salisbury Road.

Modern period (1980 – today)



- Population boom, doubling from approx. 4,000 to 8,000 between 1981 and 1991.
- Bypass road constructed during 1980's and opened in 1992, enclosing three quarters of the town periphery to south, east and north. A 'town collar' is formed changing the growth pattern.
- Suburban housing estates (typically curvilinear cul-de-sacs) developed on higher ground to north and east built between the town and bypass.
- Employment growth at Blandford Heights Industrial Estate.
- Parish boundary repositioned.

Access and integration

Replacing the Salisbury Road roundabout with a new signalised crossroad junction will address the physical barrier of the bypass to create a more integrated urban extension for Blandford through:

- Re-establishing the natural and historic growth pattern of Blandford, with a focus along Salisbury Road.
- Allowing mixed use/community facilities to be located at the most prominent, valuable and accessible location for communities (new and existing) and passing trade potential.
- Encouraging safer connections for pedestrians / cyclists linking between Blandford, the site and Pimperne.
- Requiring less infrastructure space than a roundabout and enabling greater development potential.
- Allowing the historic route alignment of Salisbury Road to be retained (avoiding an alignment bend required for a five arm roundabout approach).

The creation of new crossroads at this junction will also enable the implementation of a new three arm roundabout south of this junction on the A354, which can give direct, independent access to the core residential parcel (north east). This supports a phased approach to the delivery of new housing.

The approach proposes replacing the Sunrise roundabout with a new, larger five arm roundabout to provide access to an extended Sunrise Industrial Estate to facilitate employment led growth and the potential to relocate the household recycling centre in Shaftesbury Lane.

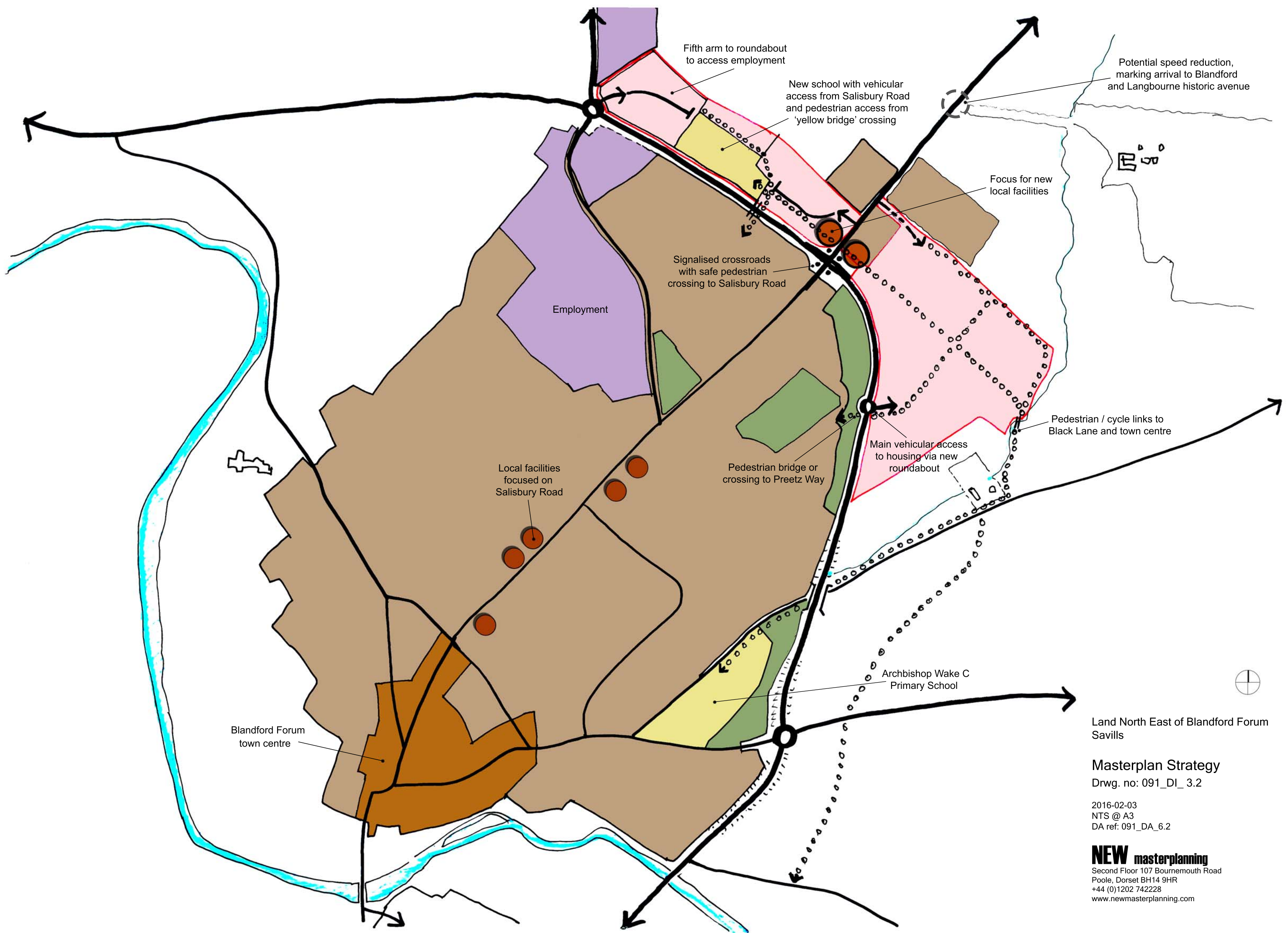
A new access junction from Salisbury Road to be created to link into the north western parcel and access a mixed-use development comprising the new school, commercial and residential uses. This requires the repositioning of most, but not all existing allotments.

A left in slip access from the A350 will enter the mixed use development focused around the Salisbury Road junction in the north western parcel.

The opportunity remains to create a new, secondary access to Salisbury Road to link into the north eastern parcel in the longer term (adjacent to ‘Bolney’), should it possible to reduce vehicular speeds on Salisbury Road and establish appropriate sight lines for the junction.

Pedestrian / cycle linkages are promoted through the site. Key connections include; crossing over the watercourse to the south west to Black Lane, over the A354 to Preetz Way and utilising the existing pedestrian crossing over the A350 (yellow bridge) to link directly to the new primary school.

Further assessment work is now required to test these arrangements. This should be progressed through a continuation of the initial discussions held with DCC highways.



Framework Masterplan

A framework masterplan has been prepared to illustrate the potential of land north east of Blandford to provide a sustainable and integrated urban extension for the town.

Land use strategy

Commercial/ Community gateway

Commercial (potential for some office, small scale food retail) and community (potential for medical / health) uses will focus on the Salisbury Road. This location will allow a stronger visual presence and increase the prospect of viable development benefiting from the significant passing trade.

This location represents the most sustainable location for these uses to encourage walking and cycling from within the new development and the adjoining existing neighbourhoods.

The mixed-use development will focus around an urban square space to mark the new, welcoming gateway to Blandford and continue the historic growth pattern of mixed use clusters focused along Salisbury Road.

Education

Approximately 3.5 hectares is proposed for a new primary school in the north west parcel. This will be sufficient space for a 2FE school that incorporates supporting recreational open space / playing fields.

The school building is repositioned slightly further to the east than previously indicated on draft plans from DCC. This seeks to encourage a stronger relationship with the existing pedestrian footbridge link over the A350, discouraging unsafe short cut routes across the main road. It also help integrate the school with the new homes and other uses to ensure it is part of the community and not an isolated space.

Vehicular access is proposed from a new junction on Salisbury Road. An attractive tree lined street will be formed, providing shared access to new mixed use development. New commercial, community and residential uses will create a positive street frontage and support safer connections to the school.

The opportunity also allows potential for pedestrian / cycle access to the school via the extended Sunrise Business Park, with an attractive green link through the adjoining landscaped space.

Employment

Approximately 4.5 hectares of land is proposed for the provision of industrial / employment uses to the far north western parcel. This facilitates the expansion of Sunrise Business Park and provides sufficient land for the potential relocation of the household recycle centre.

Landscape planting will enclose the edges of the parcel to provide a visual buffer and help integration within the wider landscape. An adjoining landscaped area of approximately 2ha will provide support Sustainable Urban Drainage (SUDs).

Residential

Approximately 17.5 hectares of residential land is proposed across the two main development parcels. The areas allow the creation of distinctive residential characters that respond positively to the site features and context whilst reflecting the built quality of historic Blandford.

An integrated new community will be created with potential to reinforce a strong identity through a planned urban village structure. Important arrival points and corners will be marked by key buildings, with new dwellings providing positive frontages to all streets and spaces.

The opportunity exists to create a more formal character in places, with grander tree lined avenue streets and crescents, where architectural references can be drawn from the Georgian character of Blandford.

Towards the site edges the development form can reflect the transition towards the countryside and the likelihood that this will represent the permanent edge to Blandford.

Allotments

The majority of the existing allotments will need to be repositioned to the north west to facilitate the creation of a mixed use gateway to Blandford and a new access street to the proposed primary school.

The masterplan proposes approximately 2.4 hectares for new allotments, which increases the exiting provision (2.2 hectares). This can help to support new facilities or increase plot provision. Further expansion potential is also shown to the west of this area, should further plots be needed to support the increase population.

Access to the plots will continue to be provided from Salisbury Road, but via a new junction and street that leads to the school. The layout ensures the site is not split requiring plots positioned either side of the access road.

The position along the northern edge will allow development to be pulled further away from the ridge edge and ‘soften’ the appearance for inward views from Pimperne village. The proposed residential street will back on to the allotments to create a secure boundary and offer natural surveillance day and night from first floor windows.

Landscape strategy

Strategic landscaping forms an integral part of the proposed masterplan. Over 12 hectares of green space, representing approximately 30% of the site, is dedicated to the creation of an integrated green and blue infrastructure network.

Central ‘Stray’

Central green strip along the alignment of the Blandford and Pimperne Parish boundary. The space will provide a visual and physical link from Salisbury Road to Black Lane and the countryside connections to the south west.

The space will enable the historic hedgerow to be retained and extended to reinstate the historic boundary.

The Stray will support a phased approach to development and help address concerns regarding the release of land through the two administrative areas governing the site.

Hedgerows

The framework masterplan seeks to retain all existing hedgerows where possible and provide appropriate offsets to support ecological strategies and ensure they are an integrated feature with easy maintenance access.

Trees

New tree planting is proposed throughout the scheme to create a ‘green and leafy’ character to the development. Key spaces will be punctuated by larger specimens to provide strong green nodes and visual reference points for inward views. This will help to break up the proposed roof scape. Appropriate offsets will be created to existing tree lines and the opportunity taken to extend the prominent tree row which extends along the south west boundary.

Landscape bunding

A landscape bund (8m wide) is proposed to line the eastern edge of the bypass. This help offer a visual and acoustic screen to the main road.

Where the site levels are lower than the bypass (along the edge extending southwards from the Salisbury Road roundabout), the development form can present a stronger built edge to the development with limited gaps or breaks to enclose the site further and offer greater screening to the road.

Drainage

The strategic landscape offers significant potential for a comprehensive SUDs network. Streets and green spaces run with the contours and feed into conveyancing corridors that run down the slopes to enter the Pimperne Brook. The drainage areas should be designed to offer dual purposes where possible to allow for recreation / play space during drier periods.

This is in line with the principles of the strategy discussed with PBA and now requires further investigation as a team.

- 1 Mixed-use local centre to define the Salisbury Road gateway to Blandford, with opportunities for community, retail and employment uses.
- 2 New signalised crossroads junction to replace the roundabout and mark a new town gateway with safer pedestrian and cycle connections.
- 3 Tree lined access street leading from Salisbury Road to the new school and addressed by new homes.
- 4 New primary school with playing fields, positioned to connect with the existing pedestrian bridge over the A350.
- 5 Existing allotments (majority) repositioned along the northern edge into a larger area with secure access and boundaries.
- 6 Green corridor supporting sustainable drainage and providing a landscaped edge to the ridgeline.
- 7 Employment expansion of the Sunrise Business Park, with opportunity to relocate the household recycle centre.
- 8 New 5th arm access from the Sunrise roundabout
- 9 New roundabout providing direct access to the proposed residential neighbourhood.
- 10 Potential pedestrian and cycle crossing to connect with Preetz Way.

- 11 Landscaped bund along the bypass edge to provide a visual and acoustic screen.
- 12 Formal Georgian character with tree lined streets, crescents and spaces.
- 13 Rural character to the site edges, with dwellings overlooking proposed green corridor routes.
- 14 Central green 'stray'. Linear green space retaining key landscape features and providing attractive pedestrian and cycle connections through the heart of the site.
- 15 Connected network of green spaces, supporting sustainable drainage and strategic planting.
- 16 Potential crossing over Pimperne Brook, delivering pedestrian and cycle connections to Black Lane and the wider countryside.
- 17 New strategic tree planting to continue the distinctive tree belt edge to the southern boundary.



Land North East of Blandford Forum Savills

Framework Masterplan
Drwg. no: 091_DI_12.0

2016-02-03
1:5000 @ A3
DA ref: 091_DA_19.0

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Sunrise
Business
Park

Badbury Heights
Community

Salisbury Road

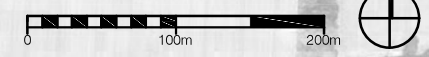
to town
centre

Preetz way

to Pimperne

Black Lane

existing Public
Right of Way



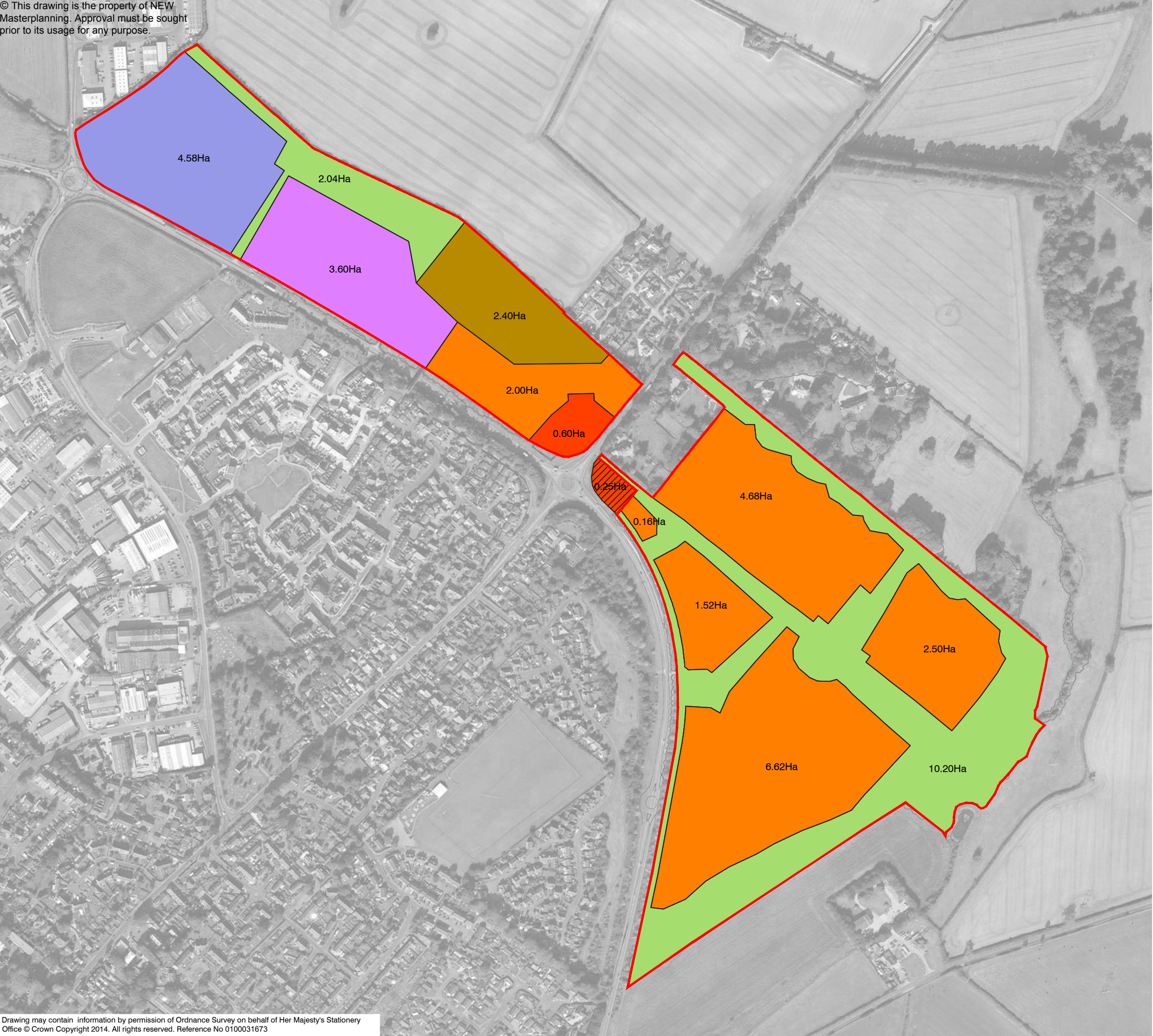
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**Pedestrian and Cycle
Connections**

Drwg: no. 091_DI_11.0

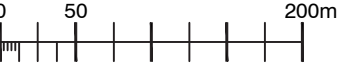
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- Employment (4.58 Ha)
- Education (3.60 Ha)
- Mixed Use (0.60 Ha)
- Housing (17.48 Ha)
- Allotments (2.40 Ha)
- Attenuation and Strategic Landscape (12.24 Ha)
- Mixed Use (0.25 Ha not in control of land owners)

Note: Area estimates based on Draft Framework Masterplan drawing ref: 091_DI_12



Land North East of Blandford Savills Land Budget Plan

Drwg. no: 091_DI_05.1

1:5000 @ A3
2016.02.03
091_DA_07.1_Land Budget Plan.dwg

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